

WEST OF RAILWAY ACTIVITY CENTRE PLAN

Precinct 2 – West of Railway

August 2021

CONTENTS

List of Figures	3
List of Tables	3
EXECUTIVE SUMMARY	4
SUMMARY TABLE	5
PART ONE – STATUTORY SECTION	5
1.0 ACTIVITY CENTRE PLAN AREA	5
2.0 ACTIVITY CENTRE PLAN CONTENT	5
3.0 INTERPRETATION AND SCHEME RELATIONSHIP	5
4.0 OPERATION	6
5.0 LAND USE AND SUBDIVISION REQUIREMENTS	6
6.0 DEVELOPMENT REQUIREMENTS	6
7.0 SITE SPECIFIC DEVELOPMENT GUIDELINES	8
7.1 Area 1 – Cornish Street (Main Street)	9
7.2 Area 2 – West of Plaza	12
7.3 Area 3 – Tudor Road.....	16
7.4 Area 4 – Forrest Road.....	19
7.5 Area 5 – Fifth Road	22
7.6 Area 6 – Gateway North.....	24
7.7 Area 7 – Gateway South	27
7.8 Area 8 – Public Open Space	31
PART 2 – EXPLANATORY SECTION	32
1.0 PLANNING BACKGROUND	32
1.1 Introduction and Purpose	32
1.2 Land Description	32
1.3 Planning Framework	34
2.0 SITE CONDITIONS AND CONSTRAINTS	37
2.1 Biodiversity and Natural Area Assets	37
2.2 Landform and Soils	37
2.3 Groundwater and Surface Water.....	37
2.4 Heritage	38
2.5 Context and Other Land Use Constraints.....	38
3.0 LAND USE AND SUBDIVISION REQUIREMENTS	39
3.1 Land Use	39
3.2 Open Space.....	42
3.3 Residential	42

3.4 Movement Networks	43
3.5 Water Management.....	44
3.6 Education Facilities	45
3.7 Activity Centres and Employment.....	45
3.8 Infrastructure Coordination, Servicing and Staging.....	45
3.9 Developer Contribution Arrangements	46

List of Figures

Figure 1: Activity Centre Plan	Page 7
Figure 2: City Centre West of Railway Precinct Plan	Page 8
Figure 3: Area 1 Cornish Street (Main Street) Subprecinct Plan	Page 9
Figure 4: Area 1 Setbacks and Heights Diagram	Page 11
Figure 5: Area 2 West of Plaza Subprecinct Plan	Page 12
Figure 6: Area 2 Setbacks and Heights Diagram A	Page 14
Figure 7: Area 2 Setbacks and Heights Diagram B	Page 15
Figure 8: Area 3 Tudor Road Subprecinct Plan	Page 16
Figure 9: Area 3 Setbacks and Heights Diagram A	Page 18
Figure 10: Area 3 Setbacks and Heights Diagram B	Page 18
Figure 11: Area 4 Forrest Road Subprecinct Plan	Page 19
Figure 12: Area 4 Setbacks and Heights Diagram	Page 21
Figure 13: Area 5 Fifth Road Subprecinct Plan	Page 22
Figure 14: Area 5 Setbacks and Heights Diagram	Page 23
Figure 15: Area 6 Gateway North Subprecinct Plan	Page 24
Figure 16: Area 6 Setbacks and Heights Diagram A	Page 26
Figure 17: Area 6 Setbacks and Heights Diagram B	Page 26
Figure 18: Area 7 Gateway South Subprecinct Plan	Page 27
Figure 19: Area 7 Setbacks and Heights Diagram A	Page 29
Figure 20: Area 7 Setbacks and Heights Diagram B	Page 30
Figure 21: Area 8 Public Open Space Subprecinct Plan	Page 31
Figure 22: Location Plan – Activity Centre Plan Area	Page 33
Figure 23: Aerial Map – Activity Centre Plan Area	Page 33
Figure 23: Context, Opportunities and Constraints Plan	Page 39
Figure 24: Indicative Concept Plan	Page 41

List of Tables

Table 1: Development Standards - Area 1 - Cornish Street (Main Street)	Page 10
Table 2: Development Standards - Area 2 - West of Plaza	Page 13
Table 3: Development Standards - Area 3 - Tudor Road	Page 17
Table 4: Development Standards - Area 4 - Forrest Road	Page 20
Table 5: Development Standards - Area 5 - Fifth Road	Page 23
Table 6: Development Standards - Area 6 - Gateway North	Page 25
Table 7: Development Standards - Area 7 - Gateway South	Page 28
Table 8: Summary of Liveable Neighbourhoods Compliance	Page 35
Table 9: Land Uses	Page 40
Table 10: Estimated Number of Dwellings	Page 43

EXECUTIVE SUMMARY

A Structure Plan was originally prepared by MW Urban planning and development consultants in 2015 on behalf of the Metropolitan Redevelopment Authority (the Authority) to guide future subdivision and land use in the City Centre West of Railway Precinct (the Precinct). The Structure Plan has been amended to promote consistency with the City Centre West of Railway Precinct Design Guidelines (Design Guidelines) and alignment with the *Planning and Development (Local Planning Schemes) Regulations 2015 - Structure Plan Framework* (WAPC Structure Plan Framework). To the extent of any inconsistencies with the Design Guidelines this ACP prevails.

The Structure Plan has been renamed as an Activity Centre Plan (ACP) to enable the inclusion of built form provisions from the Design Guidelines, reflect the Precinct's location adjacent to the Armadale Activity Centre and assist with normalisation of the West of Railway Precinct.

The purpose of the ACP is to provide a framework for the coordination of subdivision and built form development within the Precinct. To this extent, the ACP is the equivalent of a Precinct Structure Plan under the *Planning and Development Act 2005*.

The Precinct is defined in Armadale Redevelopment Scheme 2 (the Scheme) as the area bound by Green Avenue, Forrest Road and Fifth Road and has a combined area of approximately 10.84 hectares.

Consistent with objectives for the Precinct within the Scheme, the ACP facilitates predominantly medium to high density residential and mixed use development.

The main elements of the ACP are:

- retention of Tudor Road;
- extension of Cornish Street to Green Avenue;
- extension of Devonshire Terrace to the extended Cornish Street;
- development of an urban square/plaza at the intersection of Green Avenue, Cornish Street and Devonshire Terrace; and
- reduction of part of the Green Avenue road reserve, between Forrest Road and Tudor Road to 20m with the balance incorporated into the abutting fire station site.

Provision is made for an urban plaza/open space at the intersection of Green Avenue and the extended Cornish Street and Devonshire Terrace, and extending north to provide a visual link to the train station. This public space will serve as a community and business focal point for the Precinct.

The proposed movement network will improve permeability and accessibility within the Precinct, creating better links for all modes of movement between the surrounding residential areas, the train station/bus interchange and Armadale town centre. Development is to be delivered in accordance with the Scheme, the ACP and any applicable Design Guidelines and Development Policies.

West of Rail Precinct Developer Contribution Plan (DCP) Report

In 2011, the Minister for Planning approved the DCP for the Precinct, based on the approved structure plan applicable at that time. The Authority is working towards normalisation of the

Precinct, and has amended the DCP in August 2021 such that the DCP will be rescinded at the time of normalisation of the Precinct. Future arrangements for the funding of essential infrastructure required to service the Precinct will be the subject of a separate process to this ACP, and may involve the introduction of a new DCP for the Precinct administered by the City of Armadale.

SUMMARY TABLE

Item	Data	Section reference within ACP report
Gross ACP Area	10.84 hectares	1.1.2
Area of each land use proposed:		
Residential/Mixed Use	6.55 hectares = 60.4%	3.1
Roads	4.10 hectares = 37.8%	
Public Open Space	0.19 hectares = 1.8%	
Number of Primary/Secondary Schools	Nil	3.6

PART ONE – STATUTORY SECTION

1.0 ACTIVITY CENTRE PLAN AREA

The ACP applies to the area identified in Armadale Redevelopment Scheme 2 as the City Centre West of Railway Precinct, being the land identified in Figure 1: Activity Centre Plan Map.

2.0 ACTIVITY CENTRE PLAN CONTENT

The purpose of the ACP is to provide a framework for the coordination of subdivision and development within the Precinct. The ACP comprises the following sections:

- i) Section One – Statutory Section. This section includes the ACP Map and any textual provisions, standards or requirements that hold statutory weight;
- ii) Section Two – Explanatory Section (Non-Statutory). This section provides the planning context and justification for the ACP Map and the textual provisions contained in Part One of the ACP. Part Two is to be used as a reference to guide interpretation and implementation of Part One; and
- iii) Appendices, includes all specialist consultant reports and documentation used in the preparation of and to support the land use outcomes of the ACP.

3.0 INTERPRETATION AND SCHEME RELATIONSHIP

The ACP is to be read in conjunction with the Scheme, Development Policies and any applicable Design Guidelines. Unless otherwise specified, the words and expressions contained within the ACP shall have the respective meanings given to them in the Scheme including any amendments gazetted thereto.

Pursuant to clauses 5.5 and 5.18 of the Scheme, in considering a development application or plan of subdivision, the Authority is to have due regard to the provisions, standards and requirements specified under Part 1 of the ACP.

4.0 OPERATION

In accordance with clause 9.6 of the Scheme, the ACP is valid for ten years from the date of the original approval being signed, being September 2016.

5.0 LAND USE AND SUBDIVISION REQUIREMENTS

Land use shall be in accordance with Part 6 of the Scheme and Chapter 6 of the Design Guidelines (where applicable).

In respect of applications for the subdivision of land, DevelopmentWA, acting as the Authority, may recommend to the Western Australian Planning Commission (WAPC) that a condition be imposed on the grant of subdivision approval for a notification to be placed on the Certificate(s) of Title(s) to ensure that prospective purchasers are advised of:

- i) the potential for transport noise impacts generated from the nearby railway; and
- ii) the potential for quiet housing design requirements in accordance with *State Planning Policy 5.4 Road and Rail Noise* (as amended).

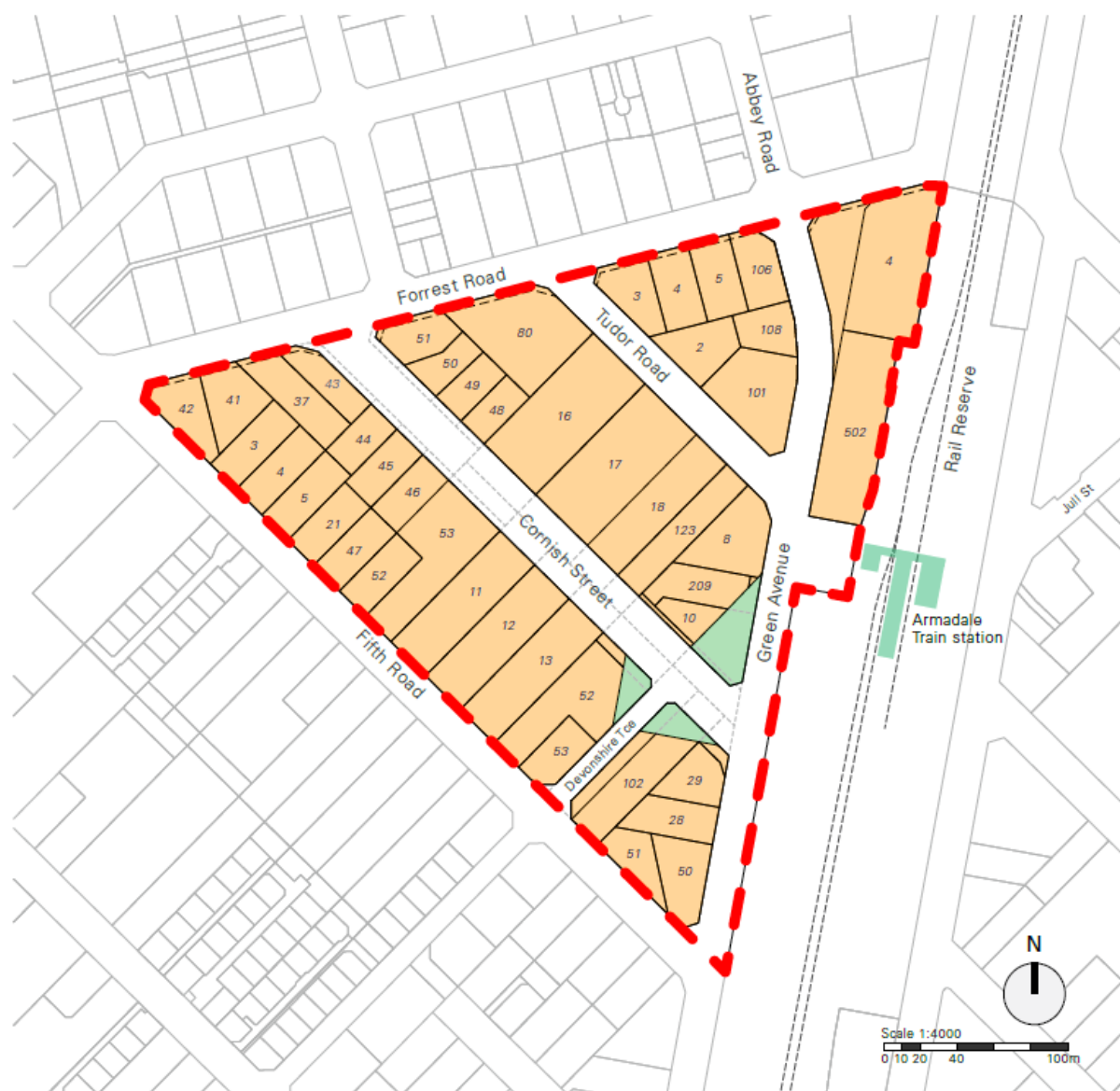
6.0 DEVELOPMENT REQUIREMENTS

Development shall be undertaken in accordance with the requirements of Part 5 of the Scheme, and any applicable Development Policies, Design Guidelines and Local Development Plan, unless specific provision is made to the contrary in this part.

Where applicable any residential development/use shall also comply with State Planning Policy 7.3 Residential Design Codes (Volume 2). Where there is any inconsistency between State Planning Policy 7.3 Residential Design Codes (Volume 2) and a provision of this ACP, Scheme, Design Guidelines or an approved Local Development Plan, the provision of the ACP, Scheme, Design Guidelines or Local Development Plan shall prevail.

In accordance with WAPC's Structure Plan Framework, an Activity Centre Plan guides the overall development (including built form) that is intended to occur within the Precinct. It can detail land use and infrastructure requirements as well as environmental assets, residential density, built form, infrastructure and access arrangements.

Figure 1: Activity Centre Plan Map



Legend



Areas 1-7



Area 8 – Public Open Space

7.0 SITE SPECIFIC DEVELOPMENT GUIDELINES

The following sections provide specific requirements at the sub-precinct level. The site specific requirements apply in addition to the General Development Guidelines.

The Development Intent outlines the design philosophy for each provision and is required to be achieved.

The Development Standards identify specific criteria which would satisfy the Development Intent, however alternative solutions for complying with the Development Intent may be considered.

A series of designated areas have been identified within the Precinct and are illustrated in Figure 3. The following areas have been defined in relation to their future land use and built form characteristics:



Figure 2: City Centre West of Railway Precinct Plan

7.1 Area 1 – Cornish Street (Main Street)

Development Intent

This portion of the precinct will be developed as a vibrant mixed use corridor and follow ‘main-street’ design principles. Cornish Street is envisioned to be a low vehicle speed, pedestrian friendly and human scaled street environment. It shall provide safe and accessible travel options for all users with an emphasis on creating places for pedestrian activity.

Buildings shall be appropriately scaled to enhance the streetscape and pedestrian experience, and designed to strengthen the local character and sense of place. Buildings shall be in a form of terrace edge mixed use apartments with compatible non-residential uses at the ground floor level providing active frontages, and visible indoor activity and passive surveillance of the public realm. Ground floor space along Cornish Street should be adaptable to allow for the conversion of dwellings to retail/commercial space over time as the latter become more viable.

Pedestrian amenity will be a key focus with weather protection to be provided over footpaths and landscaped setback areas. Interruptions to the pedestrian environment will be minimised through utilising alternative vehicle access where possible, and limiting the width and frequency of vehicle crossovers.



Figure 3: Area 1 Cornish Street (Main Street) Subprecinct Plan

Table 1: Development Standards - Area 1 - Cornish Street (Main Street)

Setbacks from Boundary	
Front	Nil setback mandatory from the road widening alignment.*
Rear	<p>7 metres minimum setback to 3 storeys up to 10.5 metres.</p> <p>Development above the 3rd storey podium shall be setback a minimum of 3 metres from the podium extent.</p> <p>Balconies may project into the rear setback area above podium subject to compliance with visual setback and private open space requirements as per Table 2 and Table 3 respectively.</p>
Side	<p>Nil setback permitted for 3 storeys up to 10.5 metres over a maximum of two-thirds the length of the boundary behind the front setback.</p> <p>4.5 metres minimum setback for any building component above 3 storeys or 10.5 metres.</p>
Building Heights (above the corresponding natural ground level)	
Minimum	3 storeys up to 10.5 metres
Maximum	5 storeys up to 16.5 metres
Minimum floor to floor height of 3.6 metres for ground floor.	
Built Form	
<p>Any building component above 3 storeys or 10.5 metres is required to be setback at 45 degrees recession plane to a maximum setback of 6m from the front façade of the lower 3 storey building component. (Refer to Figure 5).</p> <p>Weather protection shall be provided to any building fronting Cornish Street. Weather protection awnings shall have a minimum depth of 2.0 metres and shall be provided for pedestrian shelter and solar protection at ground floor level. Awning height shall provide a clear path of travel for a minimum of 2.7 metres and a maximum of 3.6 metres above the footpath level.</p>	
Site Coverage	
A maximum site coverage of 80% of the lot.	
Vehicle Access	
<p>Vehicle access from Cornish Street shall only be permitted where there is no alternative option.</p> <p>Where vehicle access is obtained from Cornish Street: Only one vehicle access point is permitted for each lot within crossover entrance to parking to have a maximum width of 6 metres at the lot boundary.</p> <p>Vehicle access shall be designed as an integrated component of all developments and to minimise visual impact on the streetscape.</p> <p>Vehicle access shall be designed so as not to impede pedestrian and cycle movement and provide safe and efficient access to the adjoining carriage way.</p>	

Where possible vehicle access should be shared between the adjoining lots to improve efficiency of site utilisation and reduce the impact of crossovers on the streetscape.

* Provision is made for Cornish Street to be widened to 28.2 metres. The width of the road widening may be subject to change having regard to future road planning and design work undertaken in the area.

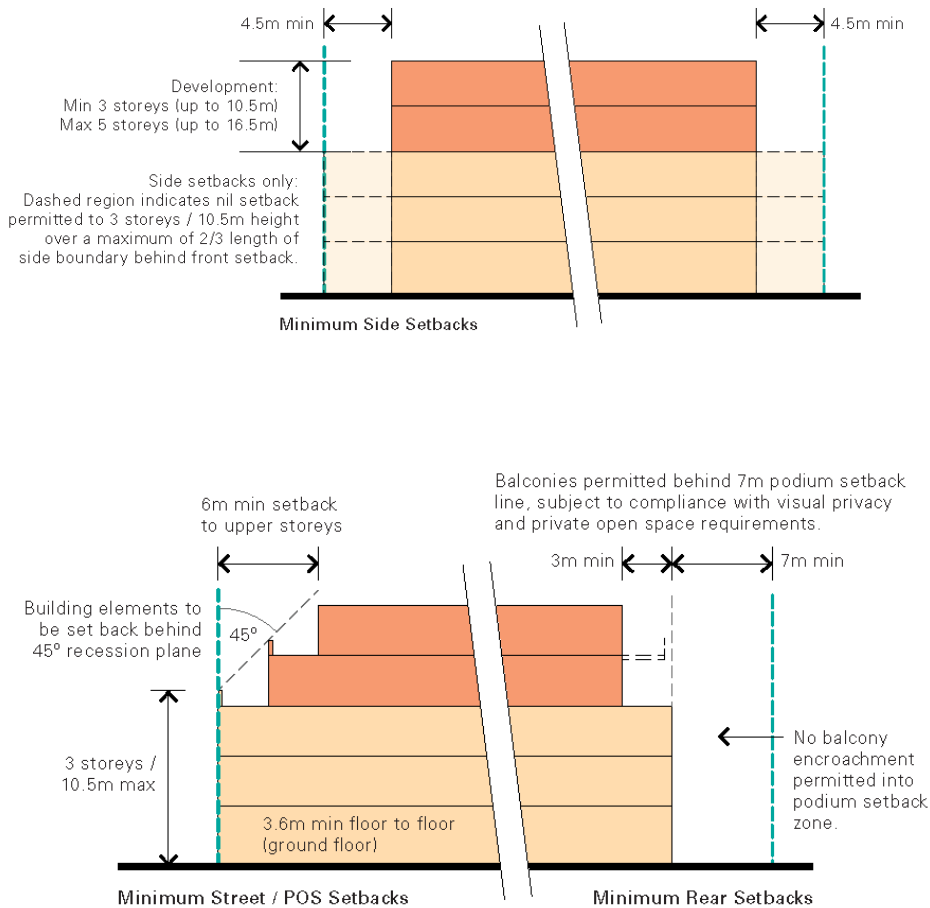


Figure 4: Area 1 Setbacks and Heights Diagram

7.2 Area 2 – West of Plaza

Development Intent

Building design shall provide a direct connection and strong link to the open space. Mixed use development shall afford good surveillance over the open space to improve safety and security for users and provide an attractive frontage when viewed from the public open space. Ground floor activity is encouraged to street frontages. Street corners shall be reinforced and could incorporate features such as a tower or turret to enhance their importance.

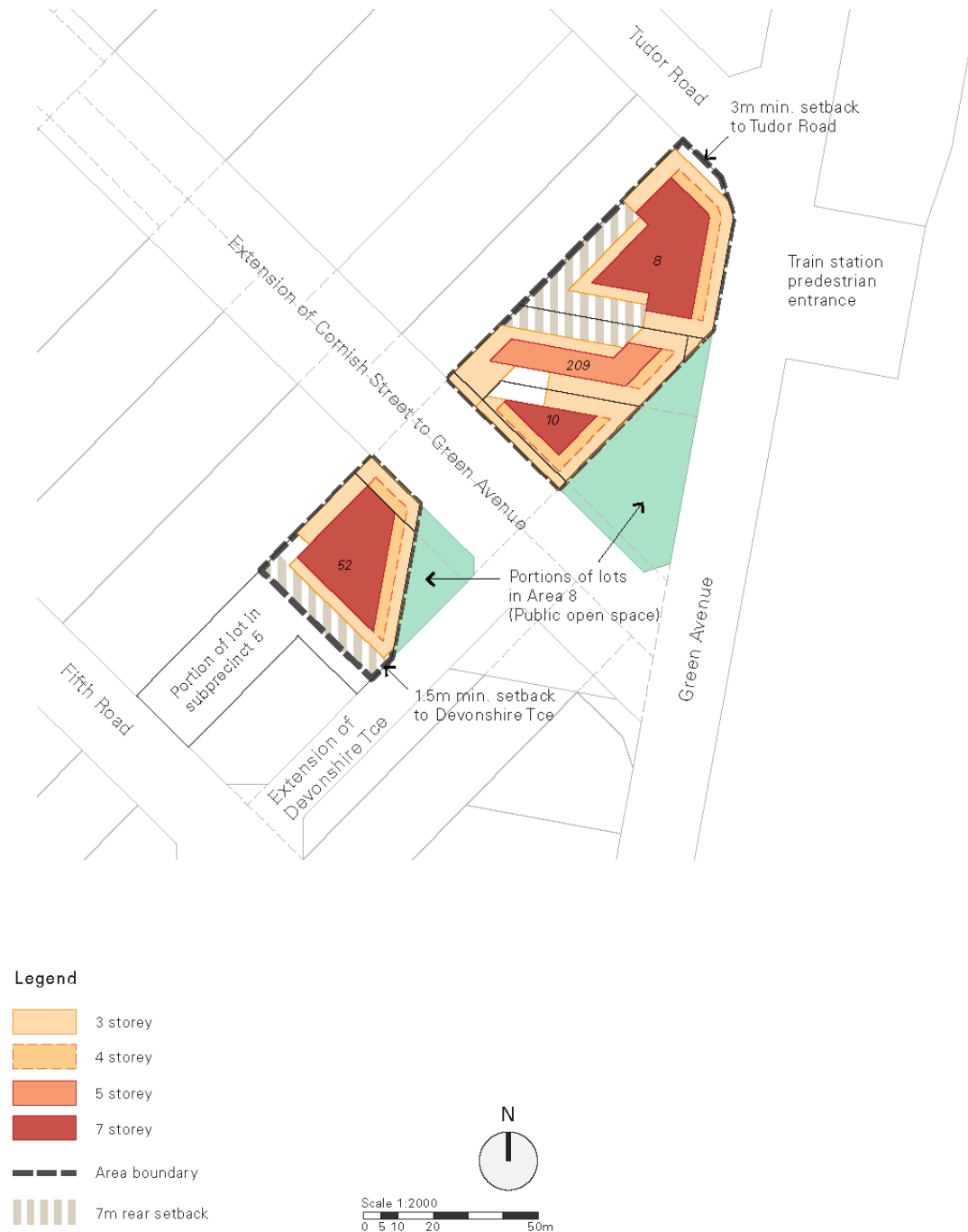


Figure 5: Area 2 West of Plaza Subprecinct Plan

Table 2: Development Standards - Area 2 - West of Plaza

Setbacks from Boundary	
Front	<p>Nil setback mandatory to Cornish Street and Open Space frontage.</p> <p>3 metres minimum to Tudor Road.</p> <p>1.5 metres minimum to Devonshire Terrace.</p>
Rear	<p>7 metres minimum setback to 3 storeys up to 10.5 metres.</p> <p>Development above the 3rd storey podium shall be setback a minimum of 3 metres from the podium extent.</p> <p>Balconies may project into the rear setback area above podium subject to compliance with visual setback and private open space requirements as per Table 2 and Table 3 respectively.</p>
Side	<p>Nil setback permitted for 3 storeys up to 10.5 metres over a maximum of two-thirds the length of the boundary behind the front setback.</p> <p>4.5 metres minimum setback for any building component above 3 storeys or 10.5 metres.</p>
Building Heights (above the corresponding natural ground level)	
Minimum	<p>5 storeys up to 16.5 metres to the following lots:</p> <ul style="list-style-type: none"> • Lot 52 Fifth Road • Lot 10 Green Avenue • Lot 8 Tudor Road <p>3 storeys up to 10.5 metres for other lots.</p>
Maximum	<p>7 storeys up to 21.5 metres to the following lots:</p> <ul style="list-style-type: none"> • Lot 52 Fifth Road • Lot 10 Green Avenue • Lot 8 Tudor Road <p>5 storeys up to 16.5 metres for other lots.</p>
Minimum floor to floor height of 3.6 metres for ground floor.	
Built Form	
<p>Any building component above 3 storeys or 10.5 metres is required to be setback at 45 degrees recession plane to a maximum setback of 6 metres from the front façade of the 3 storey building component. (Refer to Figures 7 and 8)</p> <p>Weather protection shall be provided to any building fronting Cornish Street and Open Space. Weather protection awnings shall have a minimum depth of 2.0 metres and shall be provided for pedestrian shelter and solar protection at ground floor level. Awning height shall provide a clear path of travel for a minimum of 2.7 metres and a maximum of 3.6 metres above the footpath level.</p>	
Site Coverage	
A maximum site coverage of 80% of the lot.	

Vehicle Access

Vehicle access from Cornish Street shall only be permitted where there is no alternative option.

Where vehicle access is obtained from Cornish Street:

Only one vehicle access point is permitted for each lot within crossover entrance to parking to have a maximum width of 6 metres at the lot boundary.

Vehicle access shall be designed as an integrated component of all developments and to minimise visual impact on the streetscape.

Vehicle access shall be designed so as not to impede pedestrian and cycle movement and provide safe and efficient access to the adjoining carriage way.

Where possible vehicle access should be shared between the adjoining lots to improve efficiency of site utilisation and reduce the impact of crossovers on the streetscape.

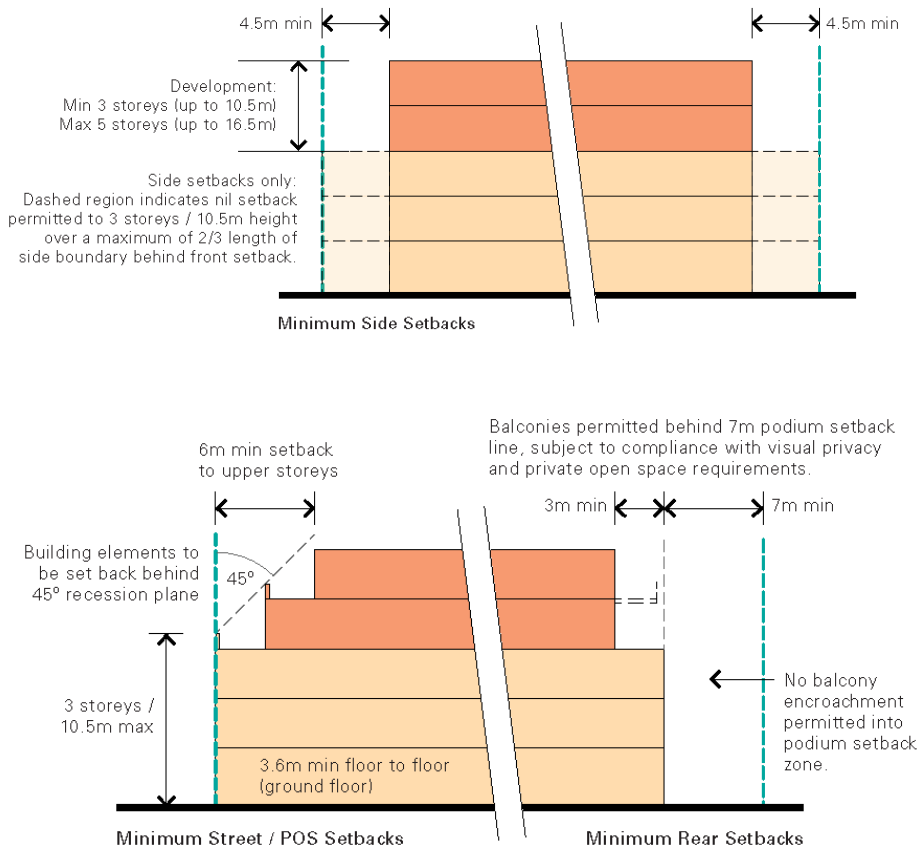


Figure 6: Area 2 Setbacks and Heights Diagram A
(Typical Lots)

ARMADALE CITY CENTRE WEST OF RAILWAY ACTIVITY CENTRE PLAN

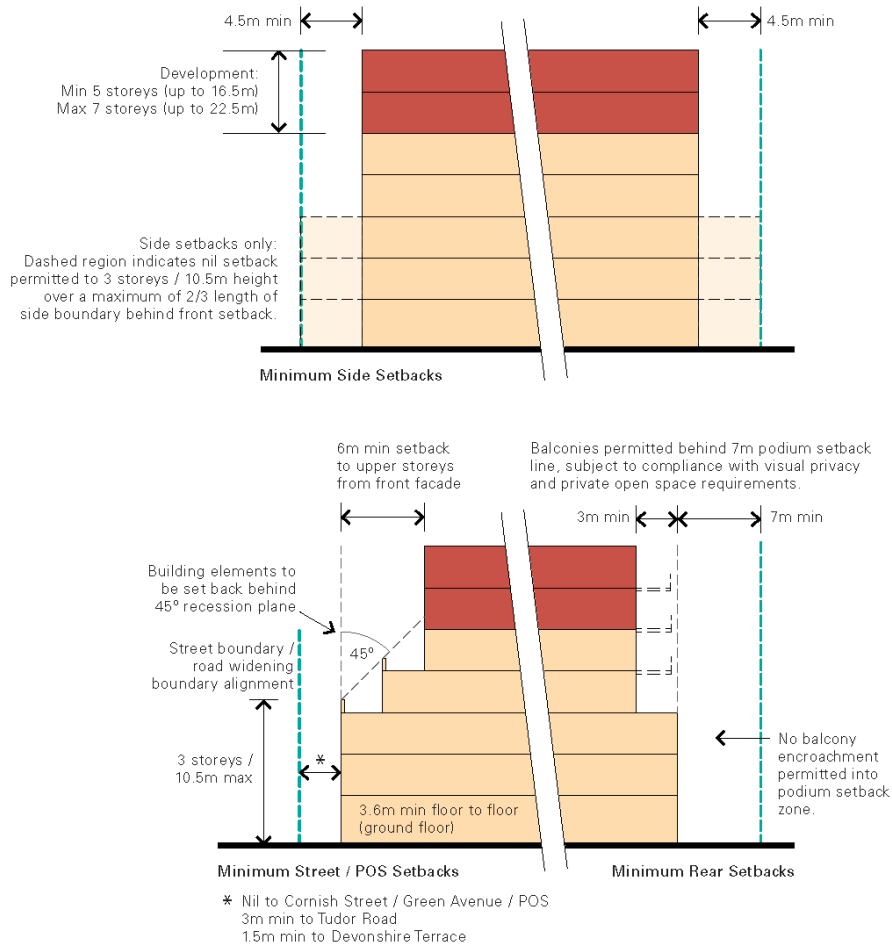


Figure 7: Area 2 Setbacks and Heights Diagram B
(Lot 52 Fifth Road, Lot 10 Green Avenue and Lot 8 Tudor Road)

7.3 Area 3 – Tudor Road

Development Intent

This area shall be redeveloped as mixed use development. New development must be of high design quality and be of scale and character which is compatible with existing development in the locality. Where possible vehicle access should be shared between the adjacent lots to improve efficiency of site utilisation and reduce the impact of crossovers on the streetscape.

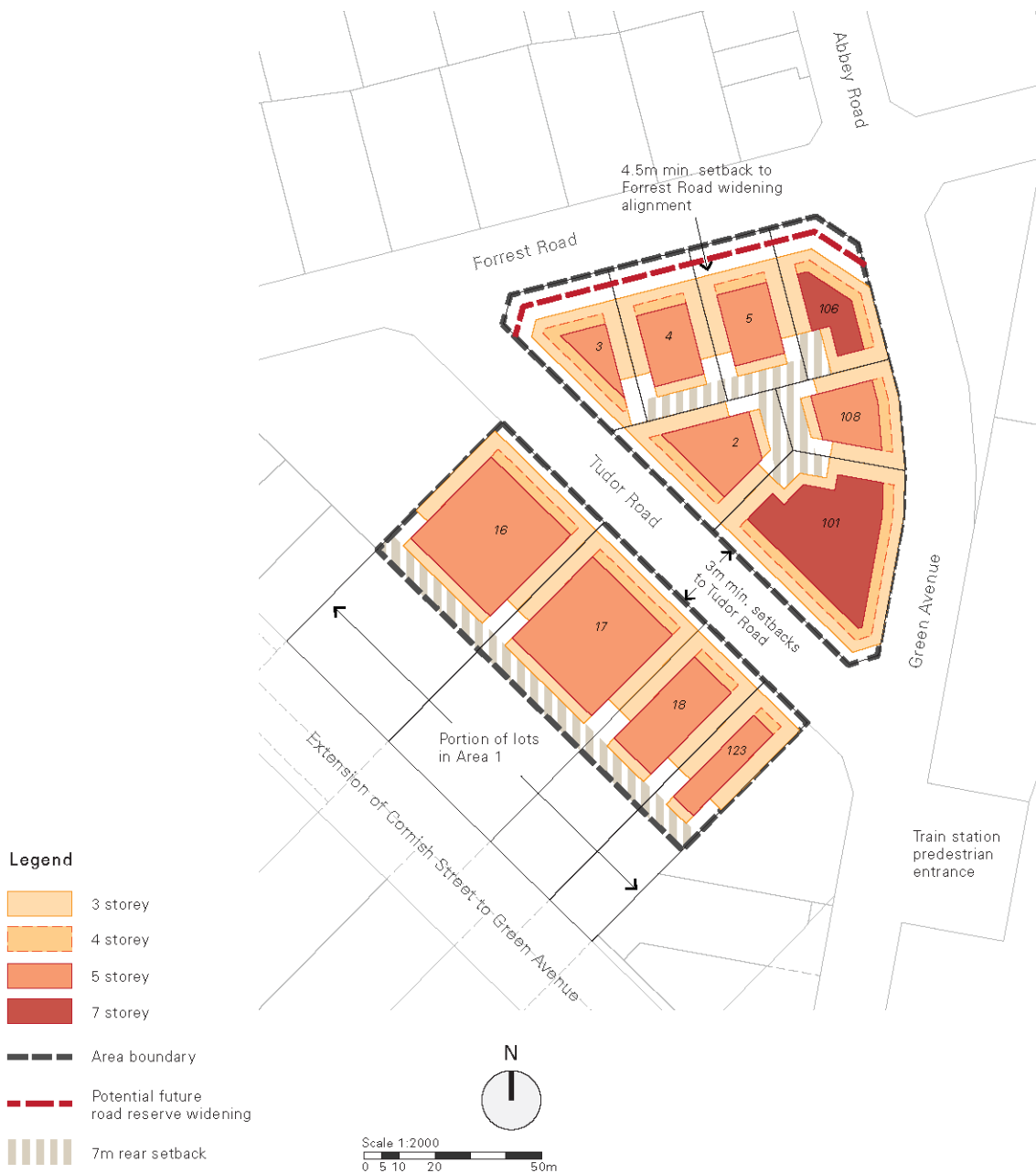


Figure 8: Area 3 Tudor Road Subprecinct Plan

Table 3: Development Standards - Area 3 - Tudor Road

Setbacks from Boundary	
Front	<p>Nil setback to Green Avenue</p> <p>4.5 metres minimum to Forrest Road widening alignment.*</p> <p>3 metres minimum to other street frontages.</p>
Rear	<p>7 metres minimum setback to 3 storeys up to 10.5 metres.</p> <p>Development above the 3rd storey podium shall be setback a minimum of 3 metres from the podium extent.</p> <p>Balconies may project into the rear setback area above podium subject to compliance with visual setback and private open space requirements as per Table 2 and Table 3 respectively.</p>
Side	<p>Nil setback permitted for 3 storeys up to 10.5 metres over a maximum of two-thirds the length of the boundary behind the front setback.</p> <p>4.5 metres minimum setback for any building component above 3 storeys or 10.5 metres.</p>
	<p>* Provision is made for Forrest Road to be widened by 4.1 metres. The width of the road widening may be subject to change having regard to future road and rail planning and design work undertaken in the area.</p>
Building Heights (above the corresponding natural ground level)	
Minimum	<p>5 storeys up to 16.5 metres to the following lots:</p> <ul style="list-style-type: none"> • Lot 101 Green Avenue • Lot 106 Forrest Road <p>3 storeys up to 10.5 metres for other lots.</p>
Maximum	<p>7 storeys up to 22.5 metres to the following lots:</p> <ul style="list-style-type: none"> • Lot 101 Green Avenue • Lot 106 Forrest Road <p>5 storeys up to 16.5 metres for other lots.</p>
Minimum floor to floor height of 3.6 metres for ground floor.	
Built Form	
Any building component above 3 storeys or 10.5 metres is required to be setback at 45 degrees recession plane to a maximum setback of 6 metres from the front façade of the 3 storey building component. (Refer to Figures 10 and 11)	
Site Coverage	
A maximum site coverage of 80% of the lot.	

ARMADALE CITY CENTRE WEST OF RAILWAY ACTIVITY CENTRE PLAN

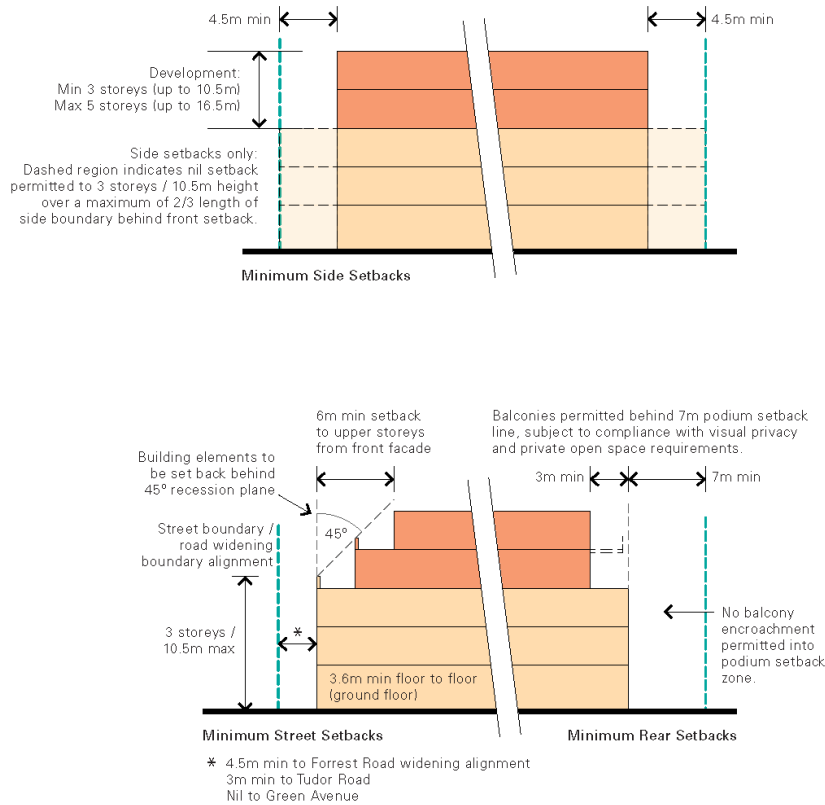


Figure 9: Area 3 Setbacks and Heights Diagram A (Typical lots)

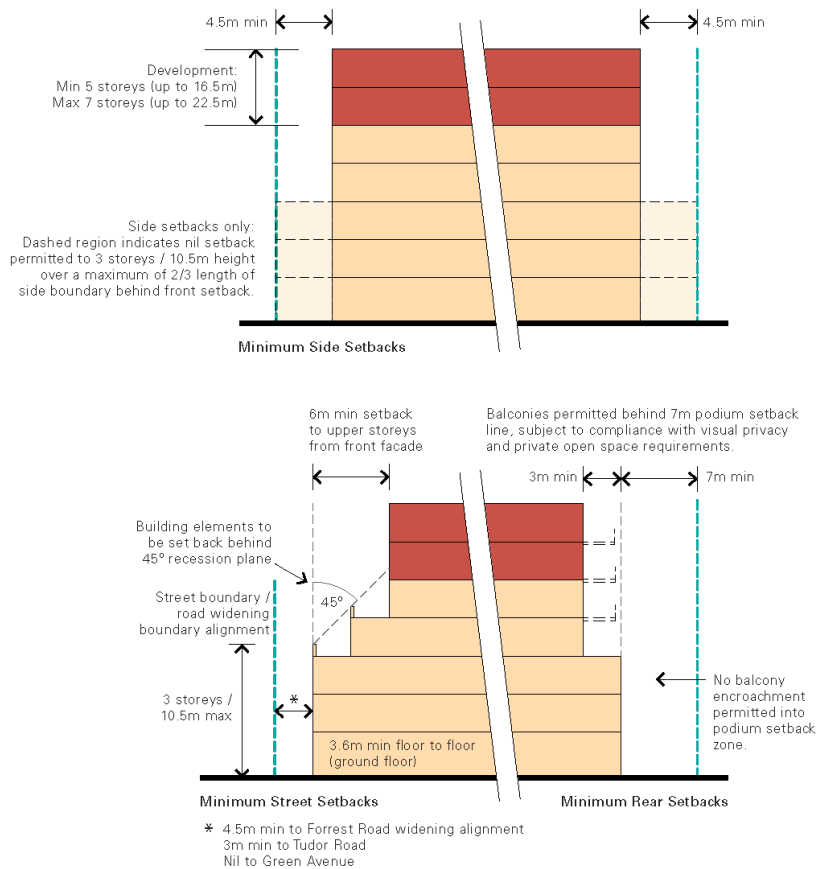


Figure 10: Area 3 Setbacks and Heights Diagram B (Lot 101 Green Avenue and Lot 106 Forrest Road)

7.4 Area 4 – Forrest Road

Development Intent

New development must be cognisant of the relationship with adjoining low density residential areas, through good design that locates building bulk appropriately, minimises blank façades, screens service infrastructure and is considerate of overshadowing and privacy concerns. The built form will assist in providing a transition from the higher intensity development at the core of the precinct towards the existing predominantly single to two storey residential development outside the redevelopment area boundary. Development along this boundary shall provide for good passive surveillance of the streets and shall address the public realm via well-presented elevational treatments.

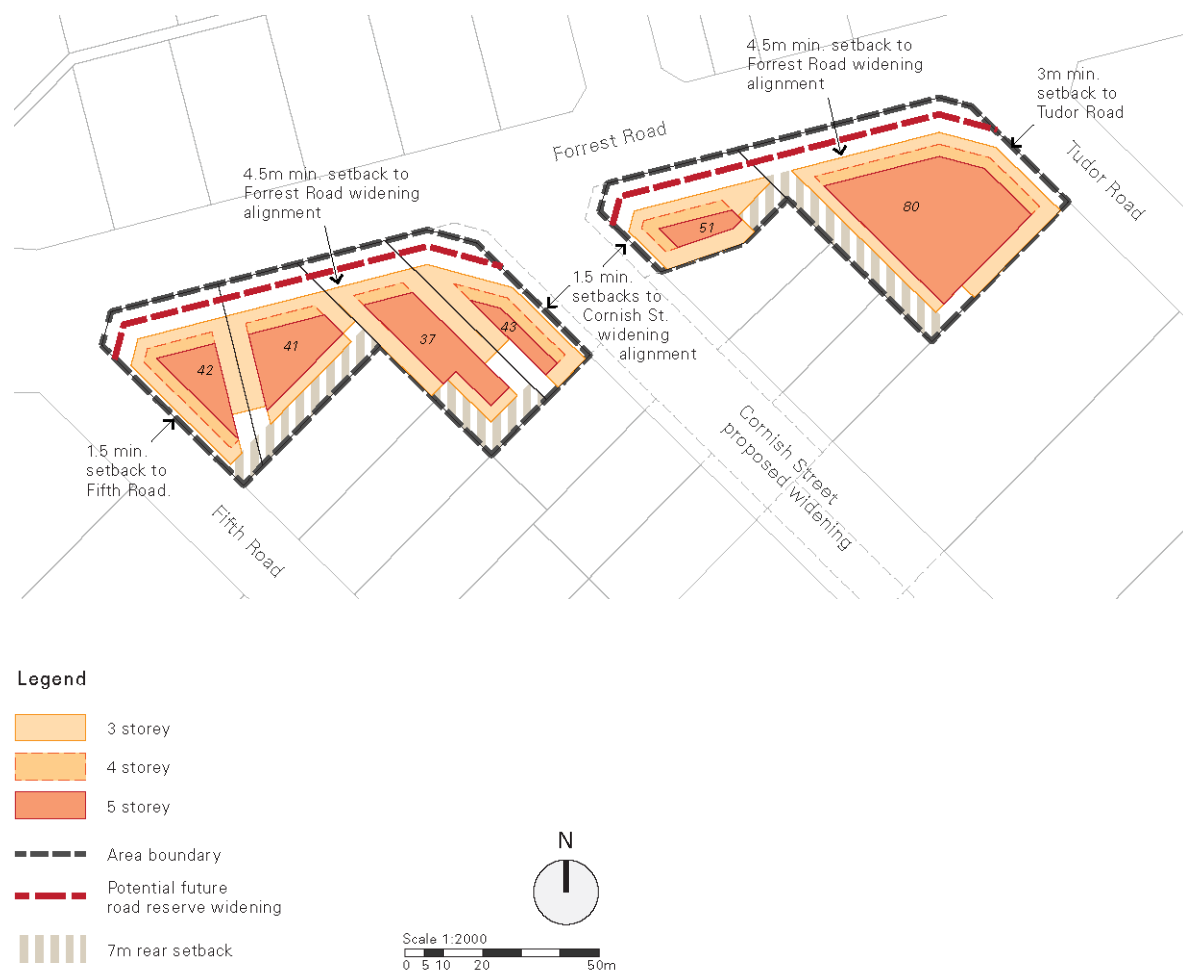


Figure 11: Area 4 Forrest Road Subprecinct Plan

Table 4: Development Standards - Area 4 - Forrest Road

Setbacks from Boundary	
Front	<p>4.5 metres minimum to Forrest Road widening alignment.*</p> <p>3 metres minimum to Tudor Road.</p> <p>1.5 metres minimum to other street frontages.</p>
Rear	<p>7 metres minimum setback to 3 storeys up to 10.5 metres.</p> <p>Development above the 3rd storey podium shall be setback a minimum of 3 metres from the podium extent.</p> <p>Balconies may project into the rear setback area above podium subject to compliance with visual setback and private open space requirements as per Table 2 and Table 3 respectively.</p>
Side	<p>Nil setback permitted for 3 storeys up to 10.5 metres over a maximum of two-thirds the length of the boundary behind the front setback.</p> <p>4.5 metres minimum setback for any building component above 3 storeys or 10.5 metres.</p>
Building Heights (above the corresponding natural ground level)	
Minimum	3 storeys up to 10.5 metres
Maximum	5 storeys up to 16.5 metres
Minimum floor to floor height of 3.6 metres for ground floor.	
Built Form	
Any building component above 3 storeys or 10.5 metres is required to be setback at 45 degrees recession plane to a maximum setback of 6m from the front façade of the lower 3 storey building component. (Refer to Figure 13).	
Site Coverage	
A maximum site coverage of 80% of the lot.	
* Provision is made for Forrest Road to be widened by 4.1 metres. The width of the road widening may be subject to change having regard to future road and rail planning and design work undertaken in the area.	

ARMADALE CITY CENTRE WEST OF RAILWAY ACTIVITY CENTRE PLAN

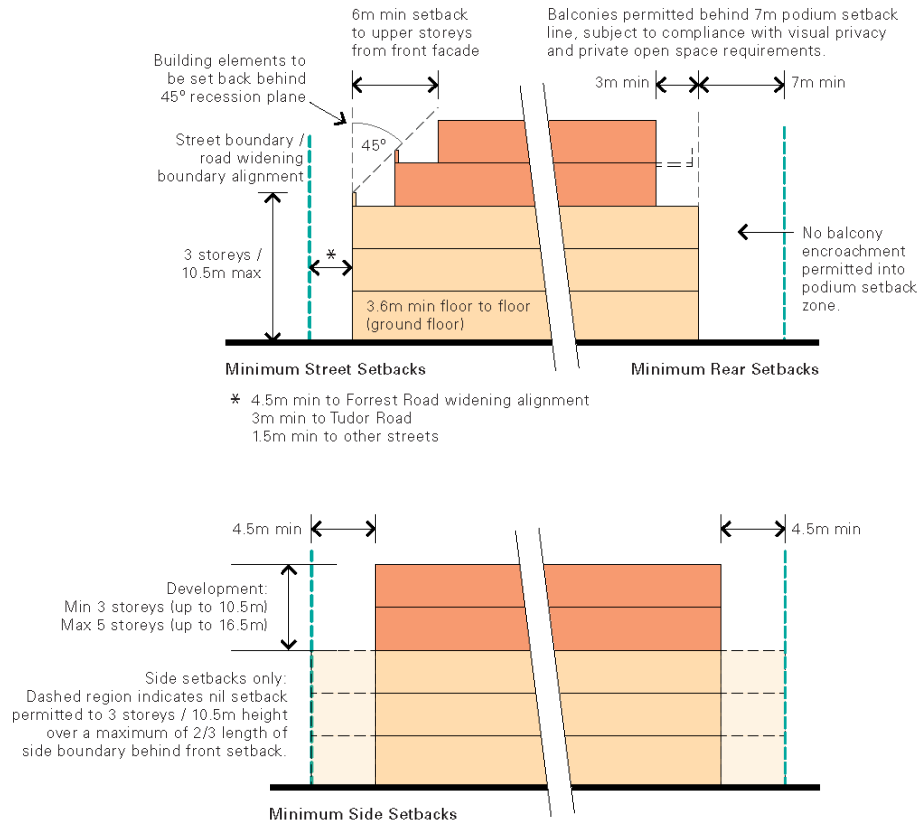


Figure 12: Area 4 Setbacks and Heights Diagram

7.5 Area 5 – Fifth Road

Development Intent

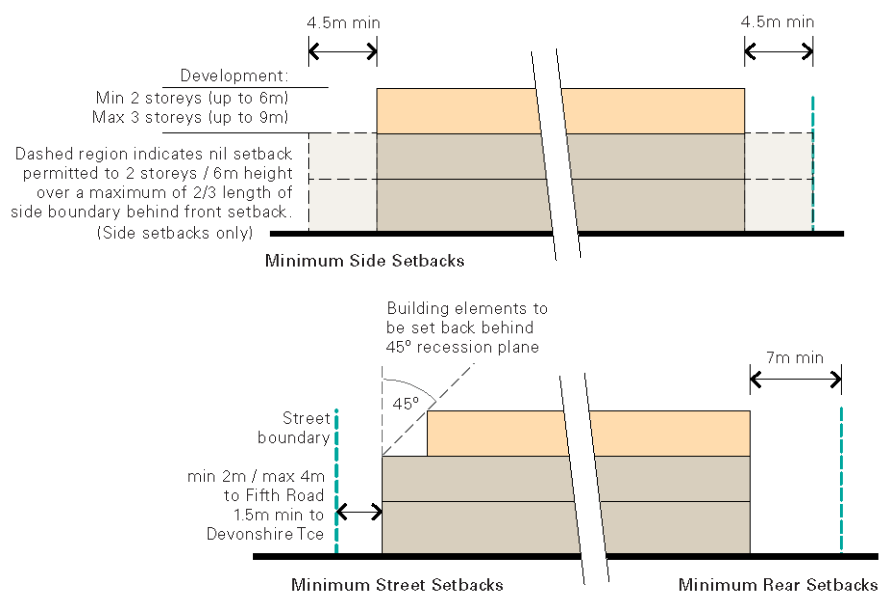
This area shall be redeveloped as residential dwellings in a terrace edge typology. The built form will assist in providing a transition from the higher intensity development at the core of the precinct towards the existing predominantly single to two storey residential development outside the redevelopment area boundary. New development must be of high design quality and be of scale and character which is compatible with existing development in the locality. Where possible vehicle access should be shared between the adjacent lots to improve efficiency of site utilisation and reduce the impact of crossovers on the streetscape.



Figure 13: Area 5 Fifth Road Subprecinct Plan

Table 5: Development Standards - Area 5 - Fifth Road

Setbacks from Boundary	
Front	For Fifth Road: 2 metres minimum 4 metres maximum. 1.5 metres to other street frontages.
Rear	7 metres minimum.
Side	Nil setback permitted for 2 storeys up to 6 metres over a maximum of two-thirds the length of the boundary behind the front setback. 4.5 metres minimum setback for any building component above 2 storeys or 6 metres.
Building Heights (above the corresponding natural ground level)	
Minimum	2 storeys up to 6 metres
Maximum	3 storeys up to 9 metres
Built Form	
Any building component above 2 storeys or 6 metres is required to be setback at 45 degrees recession plane to a maximum setback of 6m from the front façade of the lower 2 storey building component. (Refer to Figure 15).	
Site Coverage	
A maximum site coverage of 80% of the lot.	

**Figure 14: Area 5 Setbacks and Heights Diagram**

7.6 Area 6 – Gateway North

Development Intent

This site will provide an important marker at the northern approach to the precinct providing a sense of arrival from Abbey Road. Development along the northern edges of the site will form the first visible edges of the precinct and the design of the built form will set the tone for the visitors' experience of the precinct. Being located at a major road intersection and within close proximity to the train station, the site has exposure befitting a landmark building development and offers the opportunity to create an entry statement to the precinct and Armadale city centre. Development on this site shall provide for good passive surveillance and pedestrian connection to the train station and address the public realm via well-presented elevational treatments.



Figure 15: Area 6 Gateway North Subprecinct Plan

Table 6: Development Standards - Area 6 - Gateway North

Setbacks from Boundary	
Front	<p>Nil setback to Green Avenue.</p> <p>4.5 metres minimum to Forrest Rd widening alignment.*</p>
Rear	<p>7 metres minimum setback to 3 storeys up to 10.5 metres.</p> <p>Development above the 3rd storey podium shall be setback a minimum of 3 metres from the podium extent.</p> <p>Balconies may project into the rear setback area above podium subject to compliance with visual setback and private open space requirements as per Table 2 and Table 3 respectively.</p>
Side	<p>Nil setback permitted for 3 storeys up to 10.5 metres over a maximum of two-thirds the length of the boundary behind the front setback.</p> <p>4.5 metres minimum setback for any building component above 3 storeys or 10.5 metres.</p>
Rail Reserve	<p>Nil setback permitted up to 3 storeys or 10.5 metres</p> <p>4.5 metres minimum setback for any building component above 3 storeys up to 10.5m.</p>
	<p>* Provision is made for Forrest Road to be widened by 4.1 metres. The width of the road widening may be subject to change having regard to future road and rail planning and design work undertaken in the area.</p>
Building Heights (above the corresponding natural ground level)	
Minimum	<p>5 storeys up to 16.5 metres to lot on corner of Forrest Road / Green Avenue.</p> <p>3 storeys up to 10.5 metres for other lots.</p>
Maximum	<p>7 storeys up to 22.5 metres to lot on corner of Forrest Road / Green Avenue.</p> <p>6 storeys up to 19.5 metres for other lots.</p>
Minimum floor to floor height of 3.6 metres for ground floor.	
Built Form	
Any building component above 3 storeys or 10.5 metres is required to be setback at 45 degrees recession plane to a maximum setback of 6 metres from the front façade of the 3 storey building component. (Refer to Figures 17 and 18)	
Site Coverage	
A maximum site coverage of 80% of the lot.	
Vehicle Access	
Vehicle access from Lot 4 (No. 438) Green Avenue to Forrest Road shall only be permitted where there is no alternative option.	

ARMADALE CITY CENTRE WEST OF RAILWAY ACTIVITY CENTRE PLAN

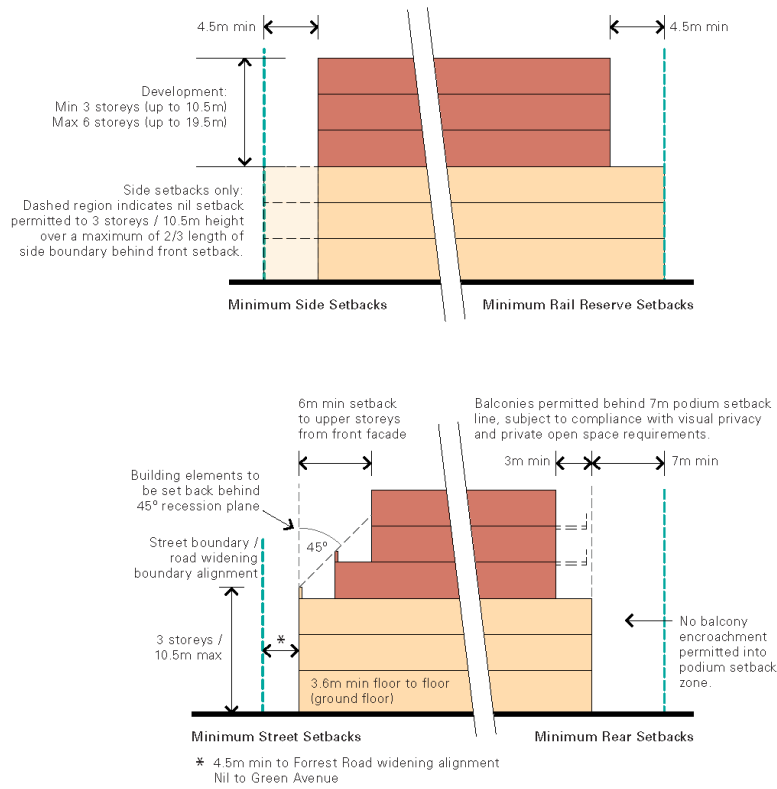
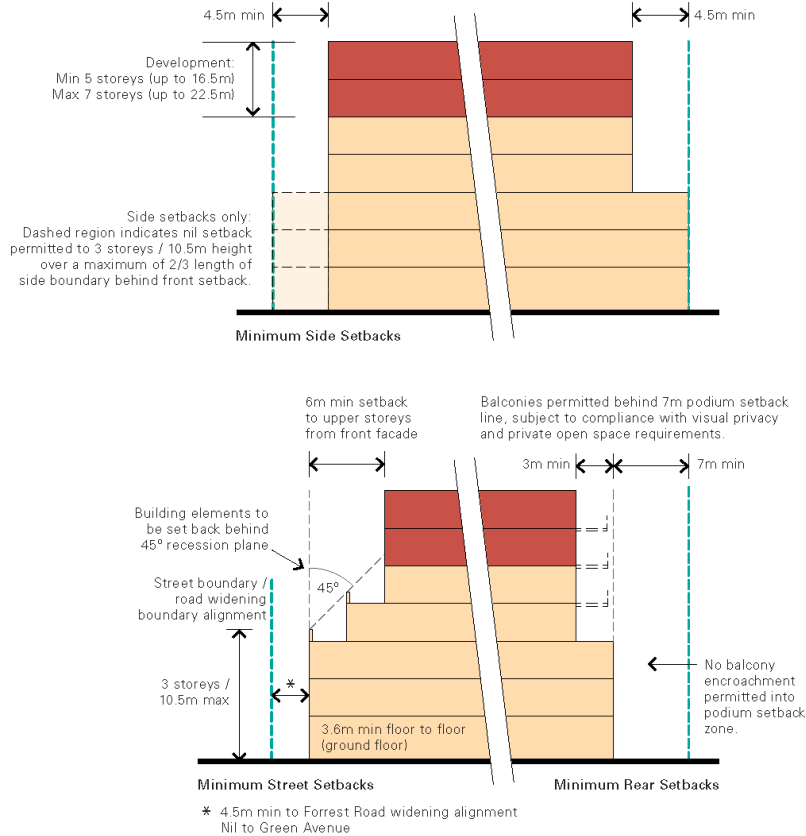


Figure 16: Area 6 Setbacks and Heights Diagram A (Typical lots)



**Figure 17: Area 6 Setbacks and Heights Diagram B
(Lot on corner Forrest Road / Green Avenue)**

7.7 Area 7 – Gateway South

Development Intent

Located at the southern edge of the precinct with its high visibility from key roads, this site plays a gateway role to mark the entry into the precinct and requires design excellence to address and define the street. To reinforce the entry point into the precinct, the built form shall be well articulated and present active uses to the street edge to provide a people presence and passive surveillance of the public realm. Elevation composition and design detail shall be considered at both the lower and upper levels, ensuring the overall design provides visual interest and presents architectural excellence to the streetscape and public open space. In addition, development along this boundary shall provide a safe and convenient pedestrian connection from the precinct to the train station.

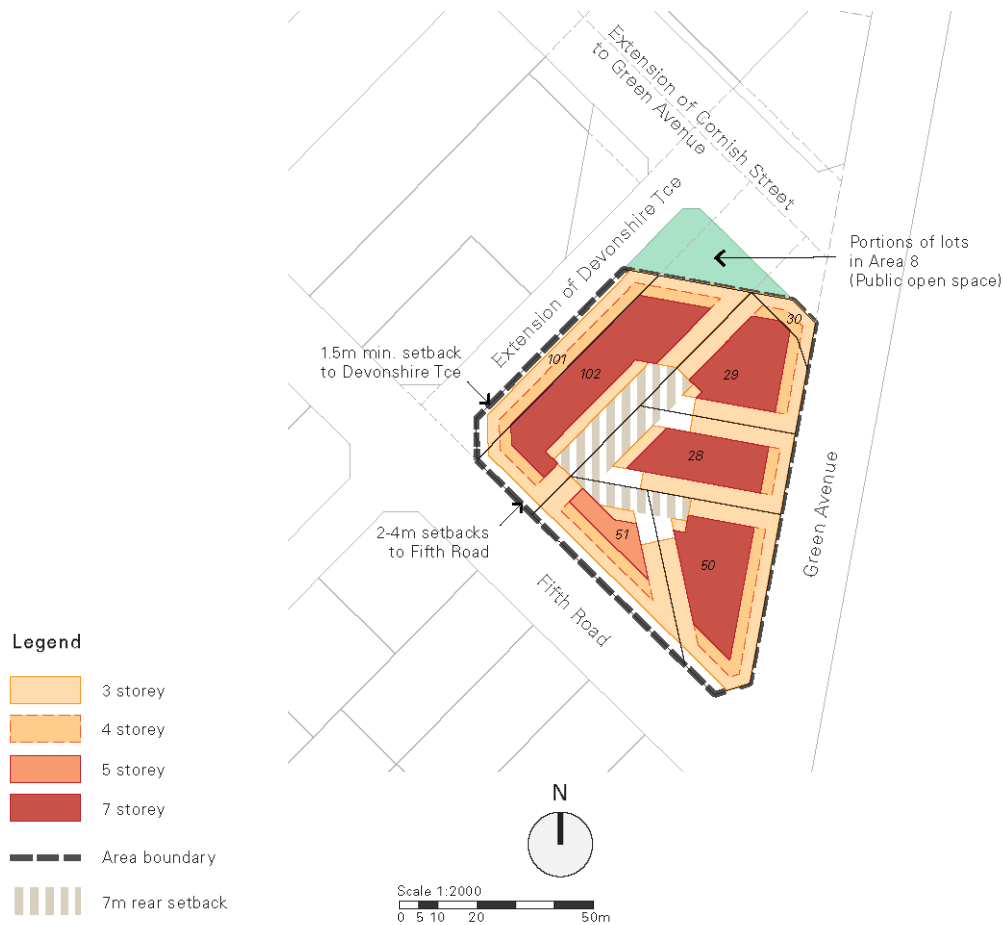


Figure 18: Area 7 Gateway South Subprecinct Plan

Table 7: Development Standards - Area 7 - Gateway South

Setbacks from Boundary	
Front	<p>Nil setback mandatory to Green Avenue and Open Space frontage.</p> <p>For Fifth Road: 2.0 metres minimum 4.0 metres maximum. 1.5 metres minimum to other street frontage.</p>
Rear	<p>7 metres minimum setback to 3 storeys up to 10.5 metres.</p> <p>Development above the 3rd storey podium shall be setback a minimum of 3 metres from the podium extent.</p> <p>Balconies may project into the rear setback area above podium subject to compliance with visual setback and private open space requirements as per Table 2 and Table 3 respectively.</p>
Side	<p>Nil setback permitted for 3 storeys up to 10.5 metres over a maximum of two-thirds the length of the boundary behind the front setback.</p> <p>4.5 metres minimum setback for any building component above 3 storeys or 10.5 metres.</p>
Building Heights (above the corresponding natural ground level)	
Minimum	<p>5 storeys up to 16.5 metres to the following lots:</p> <ul style="list-style-type: none"> • Lots 101 and 102 Fifth Road • Lot 28 Green Avenue • Lots 29 and 30 Green Avenue • Lot 50 Green Avenue <p>3 storeys up to 10.5 metres for all other lots within the sub-precinct.</p>
Maximum	<p>7 storeys up to 22.5 metres to the following lots:</p> <ul style="list-style-type: none"> • Lots 101 and 102 Fifth Road • Lot 28 Green Avenue • Lots 29 and 30 Green Avenue • Lot 50 Green Avenue <p>5 storeys up to 16.5 metres for other lots within the sub-precinct.</p>
Minimum floor to floor height of 3.6 metres for ground floor.	
Built Form	
<p>Any building component above 3 storeys or 10.5 metres is to be setback at 45 degrees recession plane to a maximum setback of 6 metres from the front façade of the 3 storey building component. (Refer to Figures 20 and 21)</p> <p>Weather protection shall be provided to any building fronting the Open Space. Weather protection awnings shall have a minimum depth of 2.0 metres and shall be provided for pedestrian shelter and solar protection at ground floor level. Awning height shall provide a clear</p>	

path of travel for a minimum of 2.7 metres and a maximum of 3.6 metres above the footpath level.

Site Coverage

A maximum site coverage of 80% of the lot.

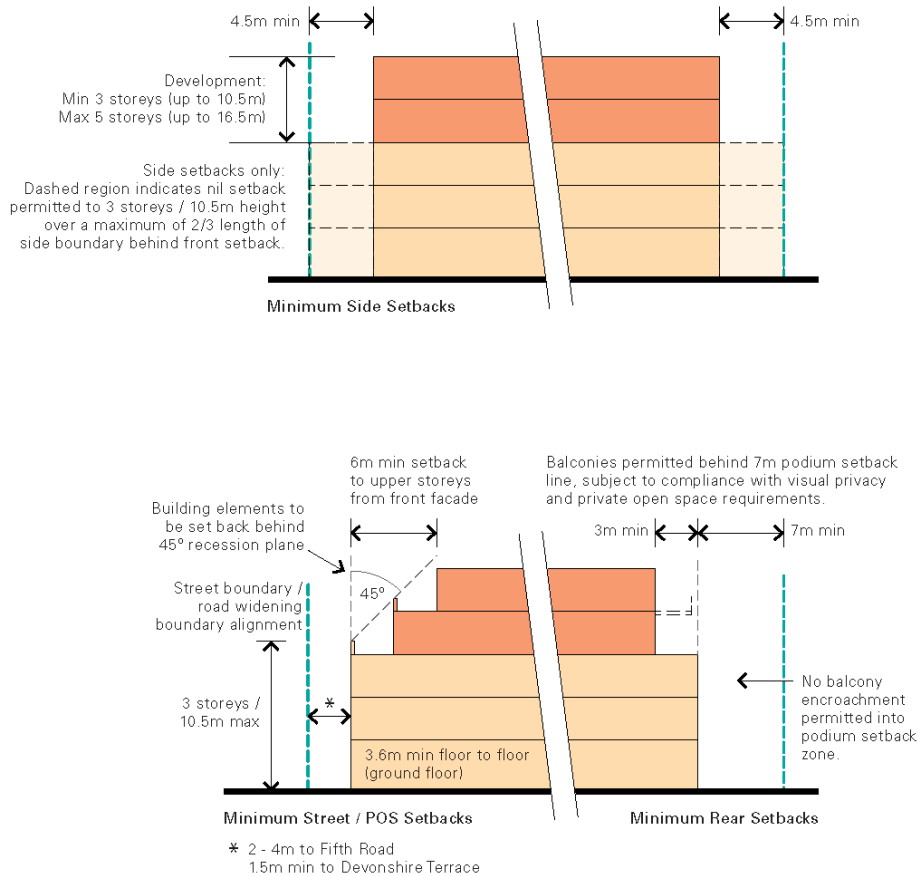


Figure 19: Area 7 Setbacks and Heights Diagram A
(Typical lots)

ARMADALE CITY CENTRE WEST OF RAILWAY ACTIVITY CENTRE PLAN

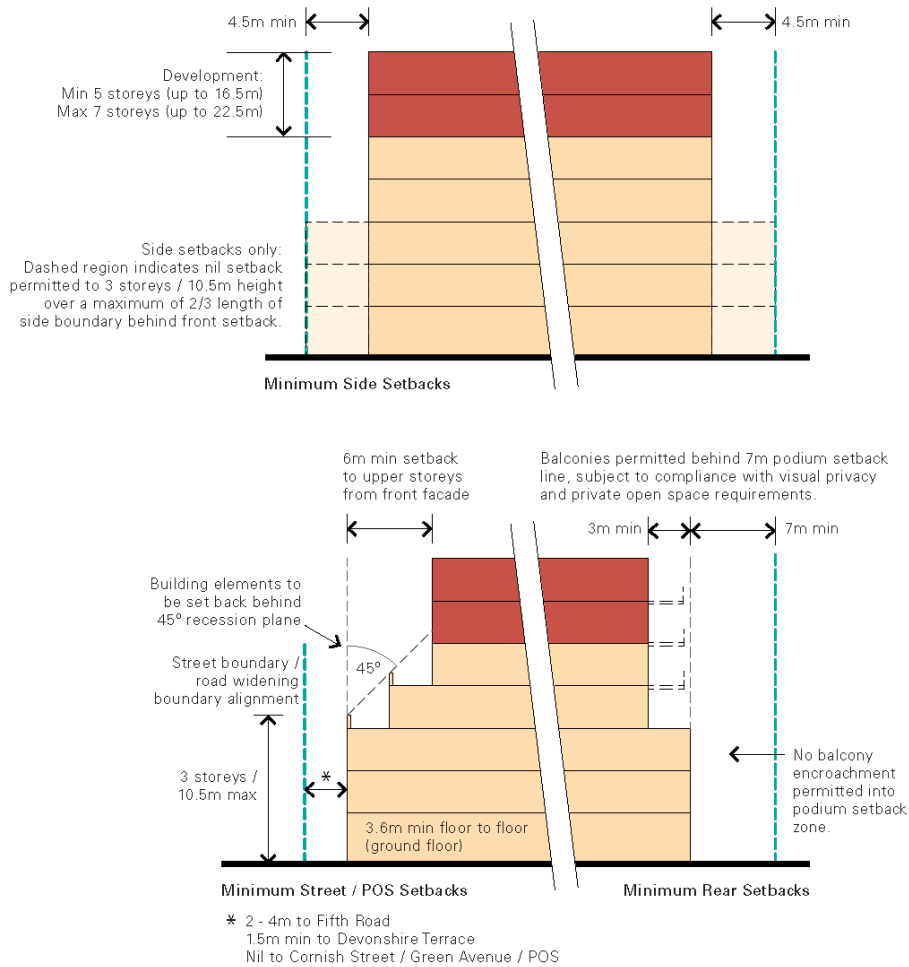


Figure 20: Area 7 Setbacks and Heights Diagram B

(Lots 101 and 102 Fifth Road - Lot 28 Green Avenue, Lots 29 and 30 Green Avenue, and Lot 50 Green Avenue)

7.8 Area 8 – Public Open Space

Development Intent

This area provides a high quality landscape and passive recreation setting for the precinct. The public open space will be a lively civic place surrounded by active uses such as cafes and restaurants and associated alfresco on the ground floor of adjacent buildings and will include high quality public facilities enabling residents, workers and visitors to use the spaces as an extension of their home.

Landscaping shall be of a high standard and contain a variety of hard and soft landscaping elements such as public seating, shade awnings or other shading devices (which may or may not be integrated with the adjacent building) and shade trees. Such elements should enhance the interaction between the open space and adjoining streets and provide a visual link to the train station.

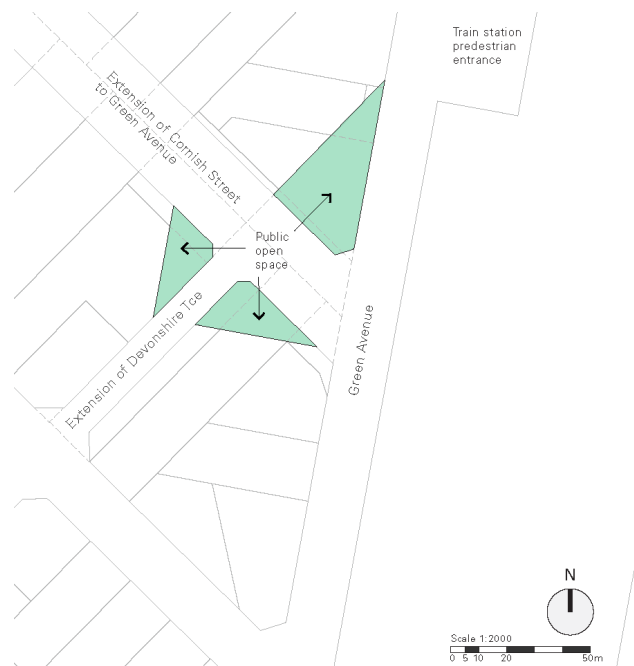


Figure 21: Area 8 Public Open Space Subprecinct Plan

Development Standards

- Design of the public open space accommodates a range of uses and activities that are complementary to the location.
- Provide high quality multi-functional public open space incorporating:
 - Children's playground equipment with adjacent sheltered seating areas.
 - Bicycle parking facilities in the public open space adjacent to active frontages of mixed use buildings.
- Crime Prevention Through Environmental Design (CPTED) principles are embedded in public open space design.
- Planting design and selection generally employs low water use and low maintenance varieties in accordance with the Water Corporation's Waterwise Development Program.
- Facilitate alfresco dining as an incidental use in a manner that contributes to activation of the space while enhancing pedestrian movement.
- A Place Activation Strategy is to be submitted as part of the Development Application.
- Safe pedestrian movement from/to the train station is to be provided.

PART 2 – EXPLANATORY SECTION

1.0 PLANNING BACKGROUND

1.1 Introduction and Purpose

This Structure Plan (2016) has been amended and renamed an 'Activity Centre Plan' to promote consistency with the Scheme and the Design Guidelines, alignment with the WAPC Structure Plan Framework and to assist with normalisation of the Precinct.

The Authority has identified the Precinct as a key redevelopment location that is ideally located adjacent to the Armadale train station, making it well positioned for Transit Oriented Development (TOD). The purpose of the ACP is to provide a framework for the coordination of subdivision and built form development within the Precinct. To this extent, the ACP is the equivalent of a Precinct Structure Plan under the *Planning and Development Act 2005*.

The ACP has been generally prepared in accordance with Part 9 of the Scheme, the WAPC's Structure Plan Preparation Guidelines and WAPC's Structure Planning Framework.

The ACP is supported by the following technical reports completed as part of the original Structure Plan, and are included as Appendices 1-4 of this ACP:

- Market Feasibility Study prepared by JLL;
- Local Water Management Strategy prepared by Emerge Associates;
- Servicing Strategy prepared by BPA Engineering; and
- Transport Impact Assessment prepared by Flyt.

In addition, the ACP is supported by the following technical report relating to the Precinct (Appendix 5), prepared in 2020 to identify sewer and gas infrastructure upgrades that may be required as the ACP area is developed:

- Sewer and Gas Scheme Amendment Considerations – Armadale West of Railway Precinct prepared by JDSI.

1.2 Land Description

1.2.1 Location

The City Centre West of Railway Precinct is located immediately west of the Armadale train station and adjacent to the established Armadale Strategic Metropolitan Centre, which is east of the railway. Figure 22 shows the location of the ACP area.

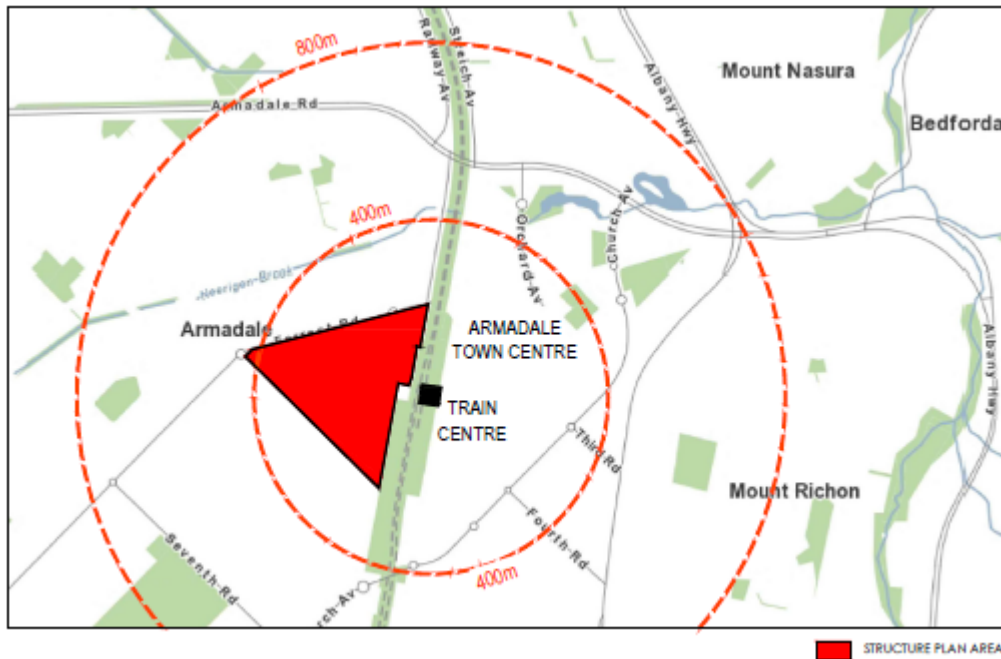


Figure 22: Location Plan – Activity Centre Plan Area

1.2.2 Area and Land Use

The ACP applies to the Precinct, as shown in Figure 23. The ACP area is a triangular Precinct of approximately 10.84 hectares, bounded by Green Avenue, Forrest Road and Fifth Road.

A number of houses occupy lots within the ACP area between Fifth Road and Tudor Road. Much of the ACP area is vacant and undeveloped, including large tracts between Fifth Road and Tudor Road, and the area bounded by Green Avenue, Forrest Road and Tudor Road.

An existing retail/showroom development is located on the southern corner of the intersection of Forrest Road and Tudor Road.



Figure 23: Aerial Map – Activity Centre Plan Area

1.2.3 Legal Description and Ownership

The ACP area comprises fragmented land that is held in multiple ownership.

1.3 Planning Framework

1.3.1 Metropolitan Redevelopment Authority

Pursuant to the provisions of the *Metropolitan Redevelopment Authority Act 2011*, the Authority is responsible for planning and development control and other functions relating to the land that is the subject of this ACP until normalisation of the Precinct. Following normalisation, the responsibility for planning and development control will be transferred to the City of Armadale and WAPC, administered under the *Planning and Development Act 2005* and subsidiary legislation.

1.3.2 Armadale Redevelopment Scheme 2

While the Authority is the responsible planning authority, the subject land is located within the Armadale Redevelopment Scheme 2 area. The ACP area comprises the whole of Precinct 2 - West of Railway, within the City Centre Project Area as identified in Figure 2.

Clause 3.1.1 of the Scheme identifies the following objectives for the Precinct:

A primary use of this precinct will continue to be residential; however the area's proximity to the train station also creates opportunities for development that benefits from being within easy walking distance of public transport facilities.

Sites closest to the train station and Armadale town centre, surrounding the urban plaza, and fronting Green Avenue, Forrest Road and the extended Cornish Street 'main street', will accommodate mixed use development incorporating compatible non-residential uses at ground level.

The movement network will significantly improve permeability and accessibility within the precinct, and create better links for all modes of movement between the surrounding residential areas, train station/bus interchange and the Armadale town centre.

Provision is made for an urban plaza/open space at the intersection of Green Avenue and the extended Cornish Street and Devonshire Terrace, extending north to provide a visual link to the train station. This public space will serve as a community and business focal point for the precinct.

Restaurants/cafes are preferred uses at ground level abutting the urban plaza to provide an active frontage to the plaza and promote natural surveillance of the space.

Table 6.2 'Land Use Table' of the Scheme identifies the land uses that are permitted within the Precinct in the Scheme area.

Part 9 of the Scheme sets out the requirements for the preparation and approval of a Structure Plan/Activity Centre Plan. Clause 9.2 of the Scheme generally requires the Authority's approval of the Activity Centre Plan prior to the Authority determining a development application or recommending approval of a subdivision application to the WAPC or the Minister for Planning for land within the Activity Centre Plan area.

1.3.3 Planning Strategies – Directions 2031

Directions 2031 was released by the WAPC in August 2010 as a spatial framework and strategic plan that guides the detailed planning and delivery of housing, infrastructure and services necessary to accommodate and manage the future growth of the metropolitan Perth and Peel region.

Directions 2031 is based on the following vision:

“By 2031, Perth and Peel people will have created a world class liveable city: green, vibrant, more compact and accessible with a unique sense of place.”

The spatial framework of Directions 2031 comprises a hierarchy of activity centres, an integrated system of public and private transport networks, and a network of parks, reserves and conservation areas. In the hierarchy of activity centres, Armadale is identified as a ‘strategic metropolitan centre’.

Directions 2031 seeks a 50 per cent improvement on current infill residential development trends and sets a target of 47 per cent of new dwellings as infill development to “make the most efficient use of available land and infrastructure.” In particular, higher residential densities are promoted in areas close to public transport, and within and near activity centres.

Consistent with the objectives of Directions 2031, this ACP facilitates the redevelopment of an urban infill site adjacent to an established strategic metropolitan centre and public transport hub.

1.3.4 Policies

1.3.4.1 Liveable Neighbourhoods

Liveable Neighbourhoods is an operational policy of the WAPC. It guides activity centre planning and the subdivision of greenfield sites, and the redevelopment of large brownfield and urban infill sites.

The following table summarises the alignment of this ACP with the Design Elements of Liveable Neighbourhoods.

Table 8: Summary of Liveable Neighbourhoods Compliance

Design Elements	Activity Centre Plan Response
Community Design	The ACP provides for the redevelopment of an urban infill area close to a strategic metropolitan centre and a public transport hub. The plan is based on a compact, walkable neighbourhood structure, including an interconnected road network that improves links to the Armadale train station and activity centre. High density residential and mixed use development is promoted.
Movement Network	The ACP promotes ‘transit oriented development’, and incorporates a safe and convenient interconnected road network that encourages walking, cycling and increased patronage of existing public transport services.
Activity Centres	High density redevelopment of this urban infill area will complement and enhance the viability of the Armadale strategic metropolitan centre.

Lot Layout	The ACP facilitates increased residential densities and mixed used development in an urban infill area close to a strategic metropolitan centre and a public transport node.
Public Open Space	While the Armadale Redevelopment Scheme 2 does not identify future public open space within the ACP, provision is made for an urban plaza at the intersection of Green Avenue and the extended Cornish Street and Devonshire Terrace as a community and business focal point for the precinct.
Education	No school sites are proposed.

1.3.4.2 Development Control Policy 1.6: Planning to Support Transit Use and Development

This ACP is consistent with the objectives of the WA Planning Commission's Development Control Policy 1.6 of promoting higher residential densities in and around town centres, and high frequency public transport nodes and interchanges.

1.3.4.3 State Planning Policy 5.4: Road and Rail Transport Noise

State Planning Policy 5.4 sets out measures and standards that are aimed at minimising the adverse impacts of transport noise. Future residential development close to the railway line will need to address this issue at development approval stage.

1.3.4.4 Armadale Redevelopment Area Development Policies

While the Authority is the responsible planning authority, pursuant to the provisions of the Armadale Redevelopment Scheme 2, the following Development Policies are applicable to the Armadale Redevelopment Area:

- Development Policy 1 – Green Building;
- Development Policy 2 – Heritage Places;
- Development Policy 3 – Sound and Vibration Attenuation;
- Development Policy 4 – Providing Public Art;
- Development Policy 5 – Additional Structures;
- Development Policy 6 – Signage;
- Development Policy 7 – Home Based Business;
- Development Policy 8 – Hosting Public Events;
- Development Policy 9 – Affordable and Diverse Housing; and
- Development Policy 10 – Adaptable Housing.

1.3.5 City Centre West of Railway Precinct Design Guidelines

Development of the ACP area is to be delivered in accordance with any applicable Design Guidelines. The Design Guidelines set out design objectives for built form and other development standards that apply to the land within the precinct.

In accordance with WAPC's Structure Plan Framework, an Activity Centre Plan guides the types of land uses and the overall development (including built form) that is intended to occur within the activity centre. It can detail land use and infrastructure requirements as well as environmental assets, residential density, built form, infrastructure and access arrangements.

The site specific built form requirements of the Design Guidelines have been included within the ACP to enable the City to be able to implement the design requirements post normalisation. To the extent of any inconsistencies, this ACP prevails.

2.0 SITE CONDITIONS AND CONSTRAINTS

2.1 Biodiversity and Natural Area Assets

The ACP area is mostly cleared and has been partly developed. There are no known existing or proposed conservation areas within the ACP area.

2.2 Landform and Soils

As noted in the Local Water Management Strategy (Appendix 2), the site ranges in height from 49 metres Australian Height Datum (AHD) to 53 metres AHD. The highest levels are seen in the north east of the site with the lowest parts of the site located along the south western boundary along Fifth Road. The site slopes steadily from the north east corner with a decreasing gradient towards the Fifth Road boundary.

The majority of the site is underlain with Guildford formation sandy-clays with colluvial gravels encroaching across the south eastern border. These soils are described respectively as:

- sandy clay – white-grey to brown, fine to coarse grained, subangular to rounded sand, clay of moderate plasticity gravel and silt layers near scarp.
- gravelly sandy clay – variable, with lenses of silt and gravel, quartz sand, subangular with eolian rounded component, heavy minerals common; gravel rounded, of colluvial origin.

The WA Atlas Acid Sulfate Soil (ASS) risk mapping classifies the majority of the site as having a moderate to low risk of ASS being present within 3.0 metres of the natural surface. A small region extending along the south eastern border of the site is classified as having 'no risk' of ASS being present within 3.0 metres of the natural surface.

2.3 Groundwater and Surface Water

As noted in the Local Water Management Strategy, soils underlying the site are largely sandy-clays with low permeability. However, due to the topography and depth to groundwater, areas of inundation are unlikely to occur within the site.

No surface water drains are located within the site. Drainage is currently located within the existing road reserves and discharges to both local government and Water Corporation infrastructure. Piped side entry drains along Fifth and Forrest Road are believed to link into the Neerigen Brook situated 100m to the north of the site running parallel with Forrest Road.

The *Perth Groundwater Atlas* indicates that minimum groundwater levels near the site are approximately 29 metres AHD. The lowest point on site is approximately 49 metres AHD and therefore depth to water is unlikely to go below 15 metres below ground level.

2.4 Heritage

There are no known places of Aboriginal heritage or European cultural heritage significance within the ACP area.

2.5 Context and Other Land Use Constraints

The ACP area is bounded by Green Avenue, Forrest Road and Fifth Road. Within the ACP area, Tudor Road links Forrest Road and Green Avenue, and Cornish Street is a cul-de-sac off Forrest Road, between Fifth Road and Tudor Road. The subject section of Forrest Road is part of a bus route linking Kelmscott and Armadale train stations (routes 243 and 244).

The ACP area is located immediately west of the Armadale train station. The established Armadale Strategic Metropolitan Centre is east of the train station. A bus interchange is situated south of the train station. The subject land is therefore ideally placed for 'transit oriented development', as the ACP area is within approximately 400 metres of the train station and bus interchange. As noted in section 1.3.4.3, the potential noise impacts of the nearby railway line will need to be addressed at development application stage.

The train station, bus interchange and surrounding land between Commerce and Green Avenues comprise Precinct 1 - Railway Station of the City Centre Project Area. Armadale Redevelopment Scheme 2 identifies the following objectives for this precinct:

Public transport will continue to be a dominant function within this precinct, focused around the railway station, with associated bus-rail interchange and park-and-ride facilities, and other uses that will help to reinforce the function of the City Centre Project Area. Development will provide a significant active street frontage on the western side of Commerce Avenue and facilitate pedestrian movement across the rail corridor. All new development will present as a minimum of two (2) storeys to the Commerce Avenue frontage. Mixed-use, residential and community uses, including continued use by the Public Transport Authority, and also cafés and other active uses will be encouraged to promote activity beyond normal business hours, improving surveillance and security around the station.

Armadale Fire Station is located on the south-east corner of the intersection of Green Avenue and Forrest Road, within the ACP area. Land between the fire station and train station has been identified by the Authority for potential apartment/mixed use redevelopment.

The immediate surrounding area to the north, south and west is predominantly residential with a density coding of R40 under TPS4. The area south of Fifth Road contains a number of grouped dwelling developments while the remainder comprises predominantly single houses. The ACP aims to improve links through the subject land to better connect the surrounding residential areas to the public transport hub and Armadale town centre.

The surrounding area contains a number of existing schools, including Neerigen Brook Primary School, Dale Christian School, John Calvin Primary School, Armadale Primary School and Armadale Senior High School. The Australian Technical College is located near the corner of Commerce and Church Avenues.

The Servicing Strategy (Appendix 3) identifies the following existing infrastructure:

- The area is currently serviced by Diameter Nominal (DN)150 PVC sewer mains discharging into a DN305VC (vitrified clay) main at the intersection of Fifth Avenue and Forrest Road.
- The existing water reticulation infrastructure comprises 100mm diameter asbestos cement and 100mm diameter cast iron water reticulation mains. A 300mm diameter ductile iron Distribution main runs along the northern side of the Forrest Road reserve.
- Gas reticulation includes:
 - 150 Steel high pressure main on 4.2m alignment along Green Avenue;
 - 100PVC medium pressure main on 1.5m alignment in Green Ave;
 - 50PVC medium pressure main on 1.5m alignment in Tudor Road;
 - 50PVC medium pressure main on 1.5m alignment in Cornish Street;
 - 100 PVC medium pressure main in Fifth Avenue.
- Western Power infrastructure includes:
 - high and voltage aerials run along the north side of Forrest Road;
 - two ground mounted transformers in the north east corner supplying the station complex and the local low voltage network;
 - it is unknown how much spare capacity is available;
 - low voltage aerials run along the south side of Fifth Road and into Cornish Street;
 - a low voltage cable runs up the west side of Green Avenue;
 - the area is supplied from Byford substation.
- Telstra infrastructure exists in all existing streets.



Figure 23: Context, Opportunities and Constraints Plan

3.0 LAND USE AND SUBDIVISION REQUIREMENTS

3.1 Land Use

Redevelopment of the Precinct will reflect and build upon Armadale's traditional urban form, enabling integration wherever possible with existing patterns of development and promoting permeability, legibility and amenity for pedestrians, cyclists and public transport users as well as private vehicles. Consistent with Armadale Redevelopment Scheme 2 objectives, the ACP

facilitates predominantly high density residential/mixed use development. Table 3 provides a breakdown of land uses.

Table 9: Land Uses

Land Use	Area
Residential/mixed use	6.55 ha
Roads	4.10 ha
Open Space (plazas)	0.19 ha
Total	10.84 ha

The subdivision pattern shall be responsive to the desired characteristics of the site and the local planning context. Any subdivision will maintain the ability for verges to accommodate mature trees and limit disruption of the pedestrian environment caused by frequent vehicle crossovers. Amalgamation of lots is encouraged to facilitate a practical and efficient layout to meet the intended development objectives.

Figure 5 shows a non-statutory, indicative concept of the Authority's vision for the ACP area. Whilst mostly outside the ACP area, the potential redevelopment of the Armadale Fire Station site and abutting land north of the train station, within the adjacent City Centre Rail Station Precinct, is also shown.



1. Traffic calmed entrance to Cornish Street.
2. Completion of Cornish Street as commercial mixed use street.
3. Angle parking to support commercial uses.
4. Central drainage swale and greenery.
5. Devonshire Terrace extended to link neighbourhood to rail and transit oriented development to local recreation fields.
6. Left in left out from Devonshire Terrace.
7. 'The Cornish' Urban green square as community and business focal point of transit oriented development.
8. North facing park and cafe plaza.
9. Park plaza visually linking 'The Cornish' and Devonshire Terrace to Rail Station.
10. Traffic calming and greening of Green Avenue.
11. Enhanced and reoriented Armadale Rail Station plaza with view to 'The Cornish'.
12. Mixed use development overlooking Armadale Rail Station plaza.
13. Apartment development.

Figure 24: Indicative Concept Plan

The main structural elements of the ACP are:

- retention of Tudor Road;
- extension of Cornish Street to Green Avenue;
- extension of Devonshire Terrace to the extended Cornish Street;
- an urban square/plaza at the intersection of Green Avenue, Cornish Street and Devonshire Terrace, and extending north to provide a visual link to the train station;

- part of the Green Avenue reserve between Forrest and Tudor Roads reduced to 20m and the balance incorporated into the abutting fire station site.

Sites closest to the train station and Armadale town centre, such as those on Green Avenue, fronting Forrest Road (a main road into the Armadale town centre), and those located on the extended Cornish Street, are likely to be the most attractive for mixed use, apartment developments incorporating compatible non-residential uses (office/commercial/retail/cafes) at ground level.

Restaurants/cafes are preferred uses at ground level abutting the public plaza at the intersection of Green Avenue and the extended Cornish Street and Devonshire Terrace, so as to provide an active frontage to the plaza and natural surveillance of the space.

The extended Cornish Street is envisaged to become the primary, mixed use 'main street' within the ACP area. Pedestrian amenity will be a key focus with weather protection to be provided over footpaths and landscaped setback areas. Ground floor space along Cornish Street should be adaptable to allow for the conversion of dwellings to retail/commercial space over time as the latter become more viable.

3.2 Open Space

Provision is made for an urban plaza/open space at the intersection of Green Avenue and the extended Cornish Street and Devonshire Terrace, and extending north to provide a visual link to the train station. This public space will serve as a community and business focal point for the precinct. North facing plaza areas will provide pleasant spaces that encourage use, including alfresco dining.

Instead of vast areas of public open space, the urban plazas and streets within the ACP area will be designed as important public spaces that encourage people to use them, consistent with the objective of reducing car dependency in this transit oriented precinct. As addressed in the Local Water Management Strategy (Appendix 2), urban water management measures will also be incorporated into the new streets instead of public open space.

3.3 Residential

Residential development will be delivered in accordance with the Scheme and Part 6 'Site Specific Development Guidelines' of the Design Guidelines where applicable.

When the original structure plan was prepared the development requirements for the precinct were mostly contained within the 'Armadale West of Railway Activity Centre Plan and Design Guideline' dated December 2010. In 2017, the Authority adopted the West of Railway Project Area Design Guidelines to deliver high quality development outcomes that meet the objectives and vision for the Precinct. The ACP has been amended to promote alignment and consistency with the existing planning framework.

It is envisaged that the primary use for the Precinct will continue to be residential, however the area's proximity to the station also creates opportunity for development that benefits from being within easy walking distance of public transport facilities. Sites closest to the train station and Armadale town centre, surrounding the urban plazas, and fronting Green Avenue, Forrest

Road and the extended Cornish Street, 'main street', will accommodate mixed use developments incorporating compatible non-residential uses at ground level.

A Market Feasibility Study is included in Appendix 1. The Study concludes that:

- "The population of the City of Armadale is anticipated to grow at a robust 4.3% per annum over the next 15 years which is approximately double the national growth rates."
- "Typical product mix in metropolitan apartment developments is circa 60% two bedroom and 40% single bedroom units. Given the perceived socio economic situation of the City of Armadale...a slight variation of 55-60% and 40-45% for two and one bedroom units would provide a relevant unit product mix. This should be balanced by...early pre market research into this aspect."

Table 10: Estimated Number of Dwellings

Dwelling Type	Area	Estimated Number of Dwellings
2 storeys	1.94 ha	161
3-5 storeys: multiple dwellings/mixed use	3.82 ha	636
Potential 5 storey apartments/mixed use*	0.79 ha	131
Total	6.55 ha	928

* Former Green Avenue road reserve, within the ACP area

Assuming an average household size of 2.5 persons, similar to that of the Cockburn Central TOD, the estimated future population of the ACP area is 2,050.

The estimated number of dwellings far exceeds the density targets of Directions 2031 and Liveable Neighbourhoods. Directions 2031 sets a target of 15 dwellings per gross urban zoned hectare of land in new development areas, which would equate to 152 dwellings for the ACP area. Liveable Neighbourhoods sets a density target of 30-40 dwellings per site hectare within 400 metres of higher order centres and train stations. Based on a residential/mixed use area of 6.55 hectares, this equates to a density target of 196-262 dwellings for the ACP area.

3.4 Movement Networks

The proposed movement network will significantly improve permeability and accessibility within the ACP area, and create better links for all modes of movement between the surrounding residential areas, the train station/bus interchange and the Armadale Strategic Metropolitan Centre.

The existing street network, consisting of Green Avenue, Fifth Road, Forrest Road, Tudor Road and Cornish Street, is to be retained. Provision is made for Forrest Road (currently 20 metres wide) to be widened by 4.1 metres within the ACP area, so that the City of Armadale's desired reserve width of 24.1 metres can be achieved. Green Avenue between Forrest and Tudor Roads will be reduced to a 20 metre reserve, with the remainder incorporated into the abutting fire station site.

Cornish Street, which is currently a cul de sac, will be widened to 28.2 metres and extended to connect to Green Avenue. The new Cornish Street will be designed as the precinct's 'main street', as well as incorporating urban water management measures such as drainage swales.

Devenish Street, which currently terminates at Fifth Road, will be extended, with a reserve of 12.6 metres to match the existing, into the ACP area to the extended Cornish Street, improving access through the ACP area between the grouped dwelling developments south of Fifth Road, and the public transport hub and Armadale town centre and beyond.

A Transport Impact Assessment (TIA) has been prepared by Flyt for the Structure Plan area and is included in Appendix 4. The TIA notes that the proximity of the Structure Plan area to a major activity centre and public transport interchange will ensure residents and visitors will have excellent transport accessibility, with the majority of nearby activities and opportunities easily accessible by walking, cycling or public transport. The assessment concludes that for traffic movements in the forecast year of 2031, projected development does not result in any significant deterioration in local road network performance.

3.5 Water Management

A Local Water Management Strategy (LWMS) is included in Appendix 2 of this ACP. The LWMS has been developed in accordance with the Better Urban Water Management framework. The LWMS has adopted an integrated water cycle management approach to managing:

- groundwater;
- surface water quality;
- surface water quantity; and
- water conservation.

An integrated water cycle management approach first assesses the existing environmental constraints and opportunities, and uses these to inform the proposed management of the key integrated water cycle elements.

The site is relatively small with existing stormwater management infrastructure upstream, within and downstream of the site. Soils underlying the site are generally low permeability and are not suited to onsite infiltration of stormwater. Further, the site discharges to both local government and Water Corporation infrastructure which have capacity limitations.

Water management within the site will be a combination of creating new infrastructure and retrofitting existing infrastructure. Lot scale water management will require all lots to provide their own detention for runoff events up to the 100 year ARI event. This water can then be either reused within the lot or later slowly released to the existing drainage network. The road network will include bio-retention/tree pit treatment areas retrofitted to existing infrastructure where possible. The new Cornish Street will also include a median swale that will provide significant bio-treatment of first flush rainfall events, stormwater detention for larger less frequent events and significant amenity. Those catchments where stormwater runoff cannot be captured and treated within bio-retention areas or the median swale will be captured and treated by a subsurface storage and treated prior to discharge to the local drainage network.

The approach described in the LWMS (and summarised above) will ensure that stormwater quality is addressed, peak flow rates do not exceed that existing capacity of the downstream drainage network, groundwater quality is protected and use of potable water is minimised.

The LWMS outlines the work to be done as a part of subdivision approval and detailed subdivision design, and provides a post-development monitoring framework that can be used to assess the effectiveness of the water management measures proposed in the LWMS.

3.6 Education Facilities

No education facilities are proposed within the ACP area, given the locality is already well served, as previously noted.

3.7 Activity Centres and Employment

The ACP provides for retail and commercial land uses immediately west of the train station, in accordance with the land use provisions of the Scheme. Such uses should complement the established Armadale Activity Centre east of the railway, which is identified as a Strategic Metropolitan Centre.

3.8 Infrastructure Coordination, Servicing and Staging

A Servicing Strategy is included in Appendix 3 of this ACP. The findings in respect to servicing the site include:

- Water Corporation has reviewed the impact of additional flows on the DN305VC sewer and advise that, it exceeds their preferred capacity and will require downstream upgrades. In addition, Water Corporation have advised that a DN225mm sewer extension is required along Forrest Road.
- It is suggested that sections of sewer running through development areas which are not on existing/proposed boundaries be relocated to the road reserve to enable full development of the sites.
- The existing sewer in the extended Cornish Street road reserve will be located in the centre of the road under the proposed median/drainage swale. Water Corporation advises that further detail will be required to determine whether the sewer can remain or whether it needs to be realigned onto a standard alignment off the boundary.
- Water Corporation advises that as part of the redevelopment of the land it would provide an opportunity to upgrade/replace the existing mains or sections of these mains.
- ATCO Gas has modelled the proposed development and based on assumptions have indicated the existing network has the capacity to supply gas to the development.
- Western Power's Network Capacity Mapping Tool shows adequate system capacity of 5Mva up to 2020.
- Upgrades and extensions will be designed by Telstra for each subdivision application and costs provided at design stage. Where road widening occurs, Telstra may require their services to be relocated to the correct alignment.

Sewer and Gas Scheme Amendment Considerations are included in Appendix 5 of this ACP, and include an updated assessment of impacts of the development sites on existing sewer

and gas infrastructure in the ACP area. The updated findings in respect to sewer and gas infrastructure are as follows:

- A DN225mm sewer extension will be required along Forrest Road between Fifth Road and Cornish Street;
- Additional flows resulting from development of the ACP area will lead to the following sections of the Forrest Road sewer flowing beyond the acceptable design capacity:
 - A 175m section of 300mm gravity collector sewer opposite the Neilson Avenue intersection; and
 - A 520m section of 375mm sewer between Markwell Avenue and Jarvis Road;
- A DN150 Steel HP (1900kPa) gas main within the western side of Green Avenue may need to be physically located prior to commencement of works that may impact the cover of the gas main; and
- Any development works which may impact the gas main should only be undertaken after it is physically located.

3.9 Developer Contribution Arrangements

In 2011, the Minister for Planning approved the DCP for the Precinct, based on the approved structure plan applicable at that time. The Authority is working towards normalisation of the Precinct, and has amended the DCP in August 2021 such that the DCP will be rescinded at the time of normalisation of the Precinct. Future arrangements for the funding of essential infrastructure required to service the Precinct will be the subject of a separate process to this ACP, and may involve the introduction of a new DCP for the Precinct administered by the City of Armadale.